

Design Review Committee 08.20.2018

#### **PROJECT TEAM**

#### Owner:

Union Square RELP Master Developer LLC (US2) 31 Union Square Somerville, MA. 02143

#### Designer:

Howeler + Yoon Architecture LLP 150 Lincoln Street, 3A Boston, MA. 02111



#### Structural Engineer:

Odeh Engineers, Inc. 1223 Mineral Spring Ave. North Providence, RI. 02904



#### **LEED Consultant:**

db, HMS 303 W Erie St, Suite 510 Chicago, IL 60654



#### **Retail Consultant:**

Graffito SP 108 Lincoln Street Boston, MA. 02111



#### Traffic Engineer:

Stantec 226 Causeway Street, 6th Floor Boston, Massachusetts 02114-2155



#### Architect:

bKL Architecture LLC 225 North Columbus Drive Suite 100 Chicago, IL. 60601



#### MEP Engineer:

Somerville, MA. 02143

R.W. Sullivan Engineers The Schrafft's City Center 529 Main St., Suite 203 Boston, MA. 02129





#### **DRAWINGS LIST**

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DRAWING LIST + CONTACTS

| CSA- C100 | CIVIC SPACE CODE        |
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## Civic Space A

UNION SQUARE SOMERVILLE, MA



UNION SQUARE RELP MASTER DEVELOPER LLC (US2) 31 Union Square Somerville, MA. 02143

#### ARCHITECT

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CONSULTANT LIST + CONTACTS

#### **DESIGN NARRATIVE**

#### A Gateway Plaza for Union Square

The arrival of transit to Union Square will establish a new civic node for the Union Square neighborhood. The proposed gateway plaza for Union Square will serve to link this node with the existing neighborhood fabric, prioritizing connectivity to Union Square Plaza through the enhancement of the public realm while maximizing its utility to celebrate the quirky, artsy, and inclusive character of Union Square.

The design approach to this public amenity has evolved with the support of community feedback and sustained coordination with the MBTA. The prioritization of pedestrian access for all users has taken center stage in the proposed concept that provides access at grade for its entire length, eliminating visual and physical barriers and replacing them with vistas that tie to Union Square Landmarks: Prospect Hill Monument, Union Square Plaza and the Historic Post Office. The placement of the plaza at grade provides the added benefit of the gradual separation of public space from Prospect Street using the natural elevation of the street to define an ample space for assembly at the point of greatest separation; a point that similarly boasts ideal southern exposure. The generous right of way, framed by green groundcover and tree canopies described herein, provides the backdrop for sustained activation by commuters, employees, residents, and visitors alike, evolving the understanding of Prospect as a Pedestrian-first street and incentivizing its utility to access the surrounding neighborhood.

The plaza is designed to be welcoming and comfortable with sun and shade and a variety of seating alternatives including both permanent accommodations and movable tables and chairs to maximize its sustained use. The spatial organization accommodates ease of movement through the space, while providing for and encouraging lingering, meeting, and even gathering for special events and/or impromptu performances. These encouraged moments of encounter are organized through a design concept rooted in history, that deliver users the length of the block from the new MBTA Station to historic Union Square Plaza and back again.

#### Recalling Miller's River

A look back at history reveals that Miller's River once passed through the D2 site and, although long disappeared, the memory of the river inspires the concept for the plaza. While not a literal tracing of the river's path, the paving pattern and overall organization of the plaza design is inspired by the dynamics and movement of Miller's River. The fluid flow of water provides a metaphor for the dynamic pedestrian connection from Union Square Station to the plaza at the heart of Union Square. Lining each edge of the plaza are multiple pockets or "eddies" of seating, steps, and other gathering opportunities. The river's edge metaphor also provides a concept for the green buffer along Prospect Street. Sloping up like a river's bank, the buffer will be planted and lined with a variety of types of seating, opportunistically penetrated from the street to encourage maximum utility.

#### **Providing More High-Quality Civic Space**

The current plan includes 25% more civic space than the plan presented at the Neighborhood Meeting. Cognizant of the desire for increased open space, building footprints fronting the plaza have been reduced through the evolution of the design to increase the amount of planned open space. The totality of the new civic space area has been designed in concert with the active and transparent retail and lobby frontages to energize the transition from station to square, while preserving view corridors and enhancing wayfinding. The massing of the D2.2 and D2.3 buildings peel back from Prospect Street, affording visibility to the station and leaving a generous green buffer between the plaza and street. The landscape edge at the street and the building facades at grade are inflected in a dynamic, sculptural way that together define an 'outdoor room' that offers pockets of space for different types of activity the length of the plaza.

#### **Multiple and Varied Activity Zones**

The plaza has been organized to accommodate ease of movement to the station as well as for comfortable everyday uses and the special events that are so characteristic of Union Square. A clear central zone, paved in a smooth material, provides a direct connection to the station. On either side of this movement zone are opportunities for seating, gathering and other activities. The planted "bank" along Prospect Street is lined with benches and seatwalls that bend in and out creating pockets of seating with shifting orientations. A number of free standing planting beds also edged with seatwalls, provide additional places to linger in the sun and the shade. Throughout the plaza moveable tables and chairs offer places to sip a coffee, chat with a friend or work on a laptop outside. A set of seating steps descending from Prospect Street at the southern end, the widest and sunniest part of the plaza, offers a spot to watch a performance or participate in a public event, all while keeping an eye on train arrivals and departures.

#### **Connections for Pedestrians and Bicycles**

The new plaza is situated at the level of Union Square Station, providing an easy and accessible connection to the heart of Union Square to the north. The western edge of the civic space meets the sloping sidewalk of Prospect Street in three different locations: at the northernmost end – connecting to Prospect Street at grade, midway - through a low sculptural stair, and at the southernmost edge - with a south facing seating stair. These penetrations encourage utility and sustained activation of the public space. Pedestrian sidewalks and bicycle routes on either end of the D2.2 and D2.3 buildings connect the plaza to sidewalks and bike routes to the east. Ample accommodations for both short term and long-term bicycle parking will be provided, as well as sheltered and secure MBTA parking to encourage bicycle use.

## Civic Space A

UNION SQUARE SOMERVILLE, MA



OWN

UNION SQUARE RELP MASTER DEVELOPER LLC (US2) 31 Union Square Somerville, MA. 02143

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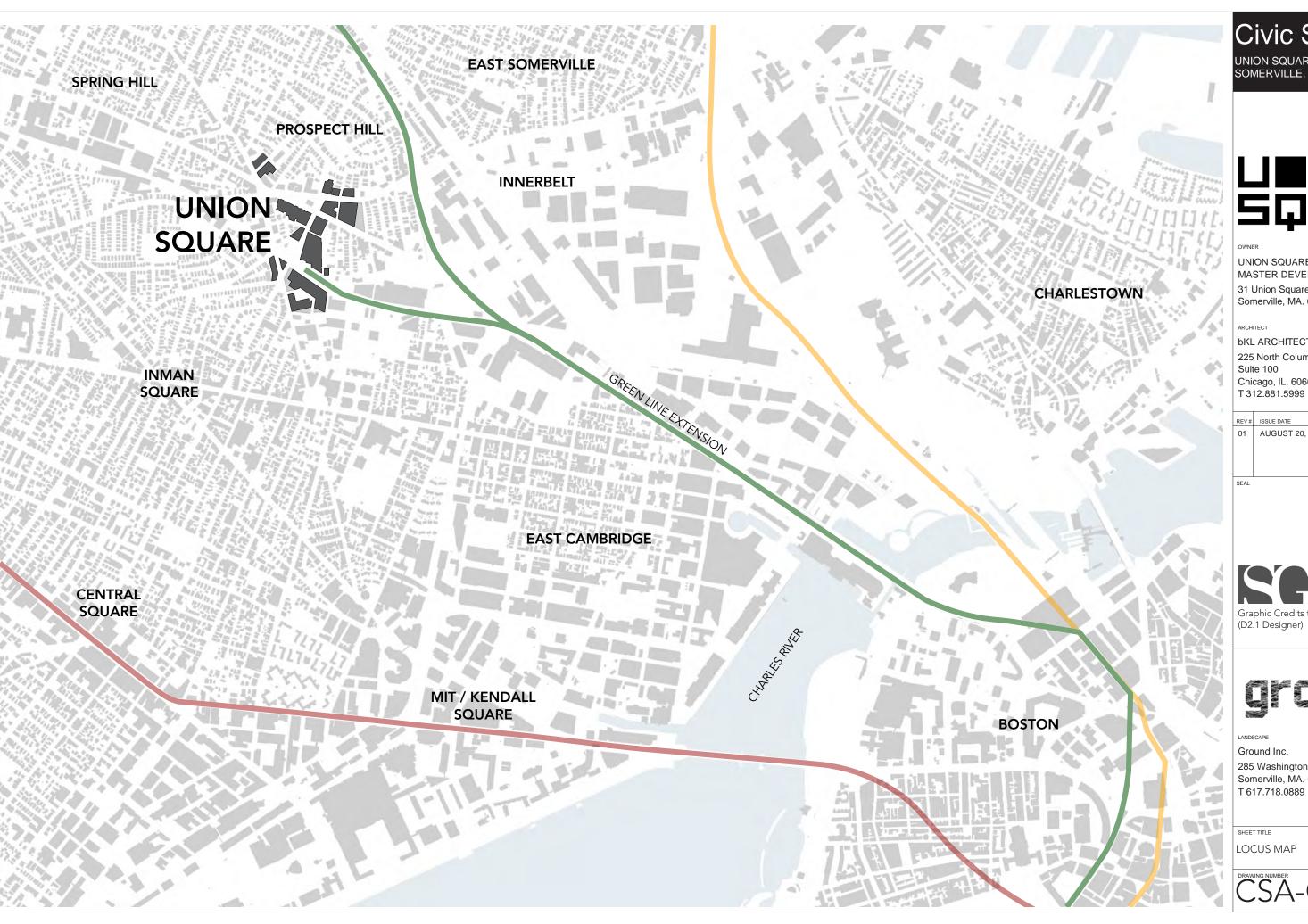
LANDSCAPE

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SHEET TITLE

DESIGN NARRATIVE



UNION SQUARE SOMERVILLE, MA



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(D2.1 Designer)



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LOCUS MAP



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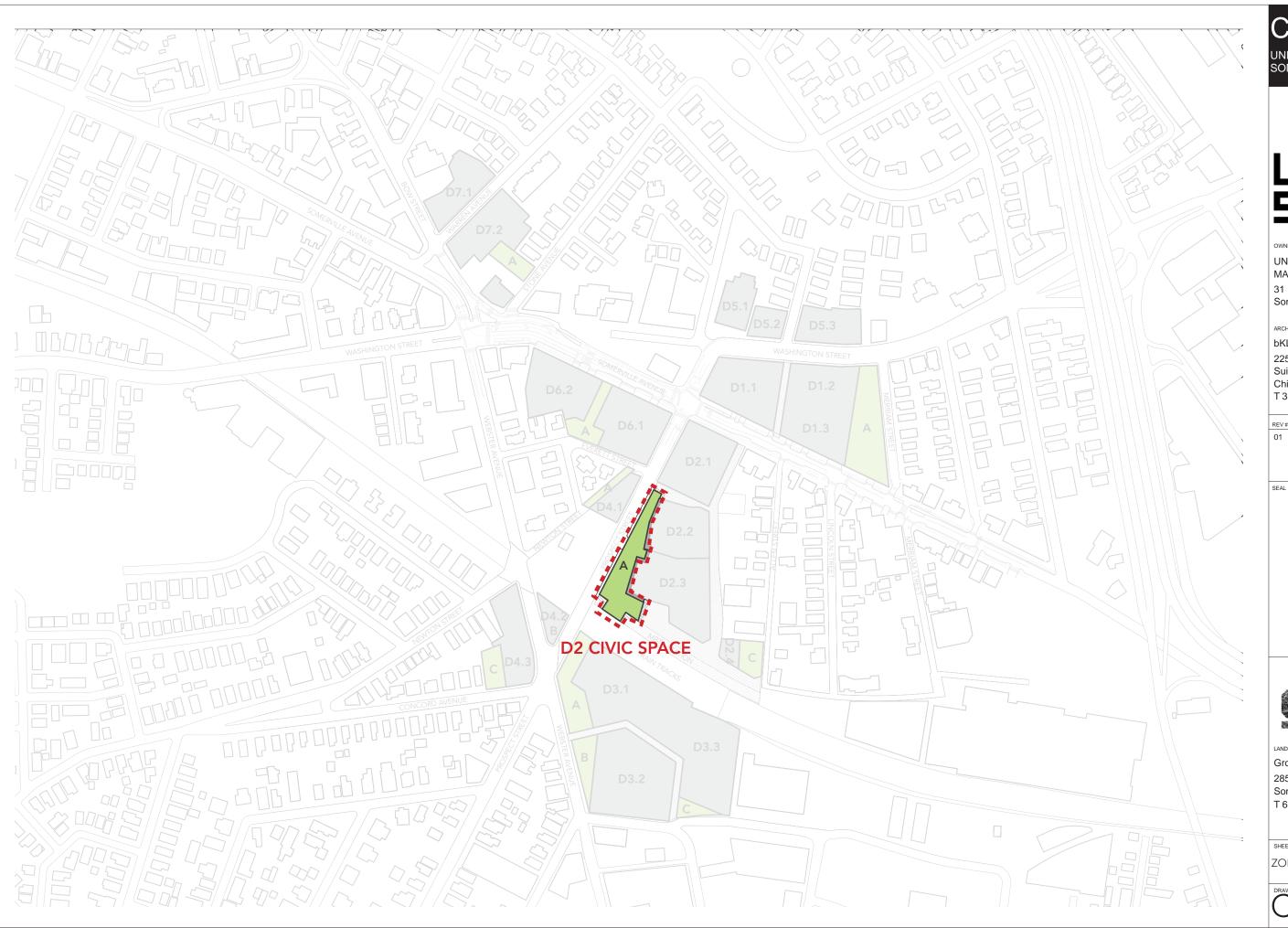
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SHEET TITLE

ZONING MAP



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ZONING MAP



- A View from Prospect Street to civic space
- B View from bridge to civic space
- C View from civic space to the bridge
- D View to train track
- E View from Alley B facing South
- F View from Allen Street home facing D2













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CONTEXT PHOTOGRAPHS



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CONTEXT PHOTOGRAPHS



### Aerial view looking south east towards Downtown Boston

## Civic Space A

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Graphic Credits to Howeler + Yoon Architecture (D2.3 +D2.3 Designer)



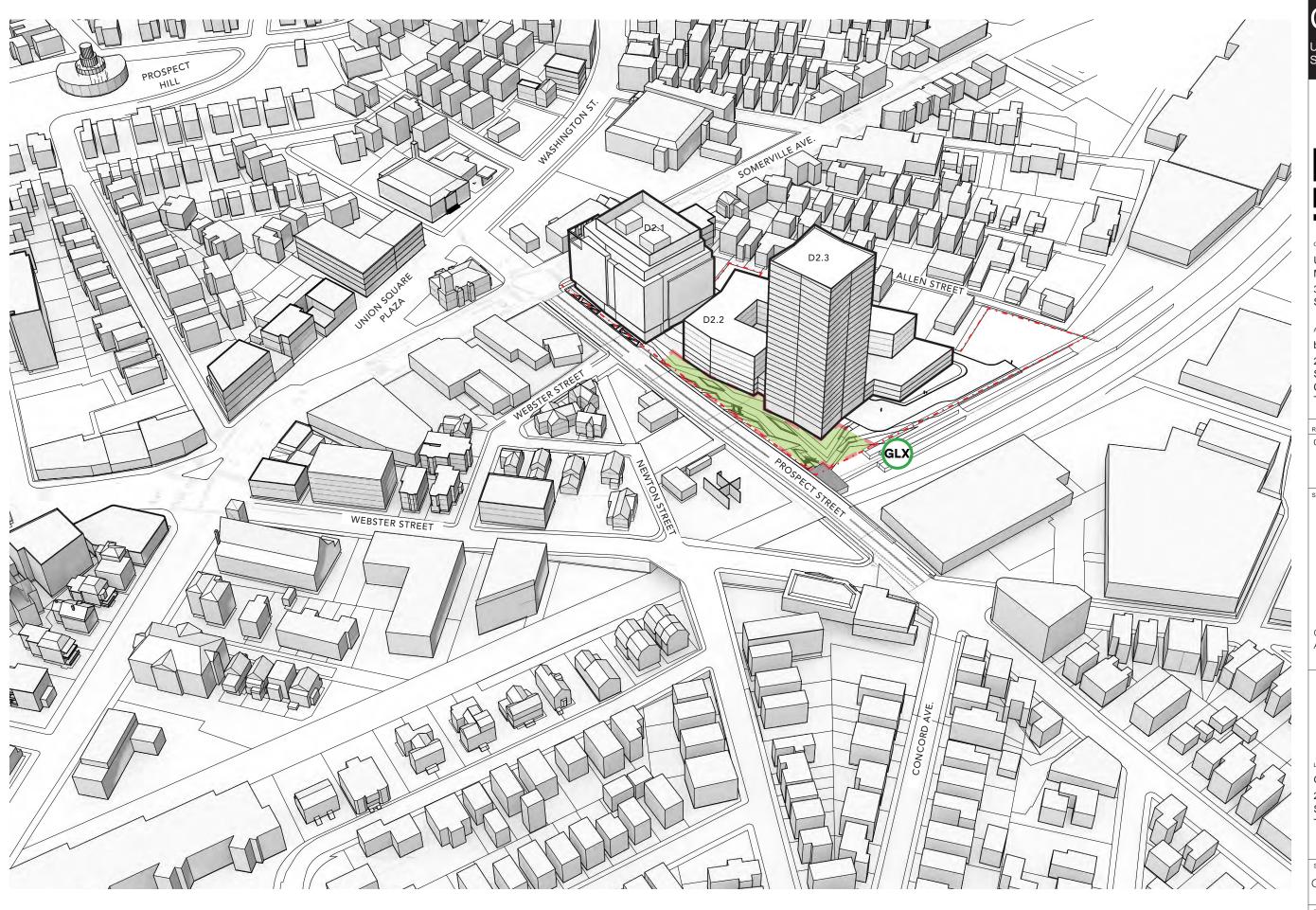
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CONTEXT PERSPECTIVE



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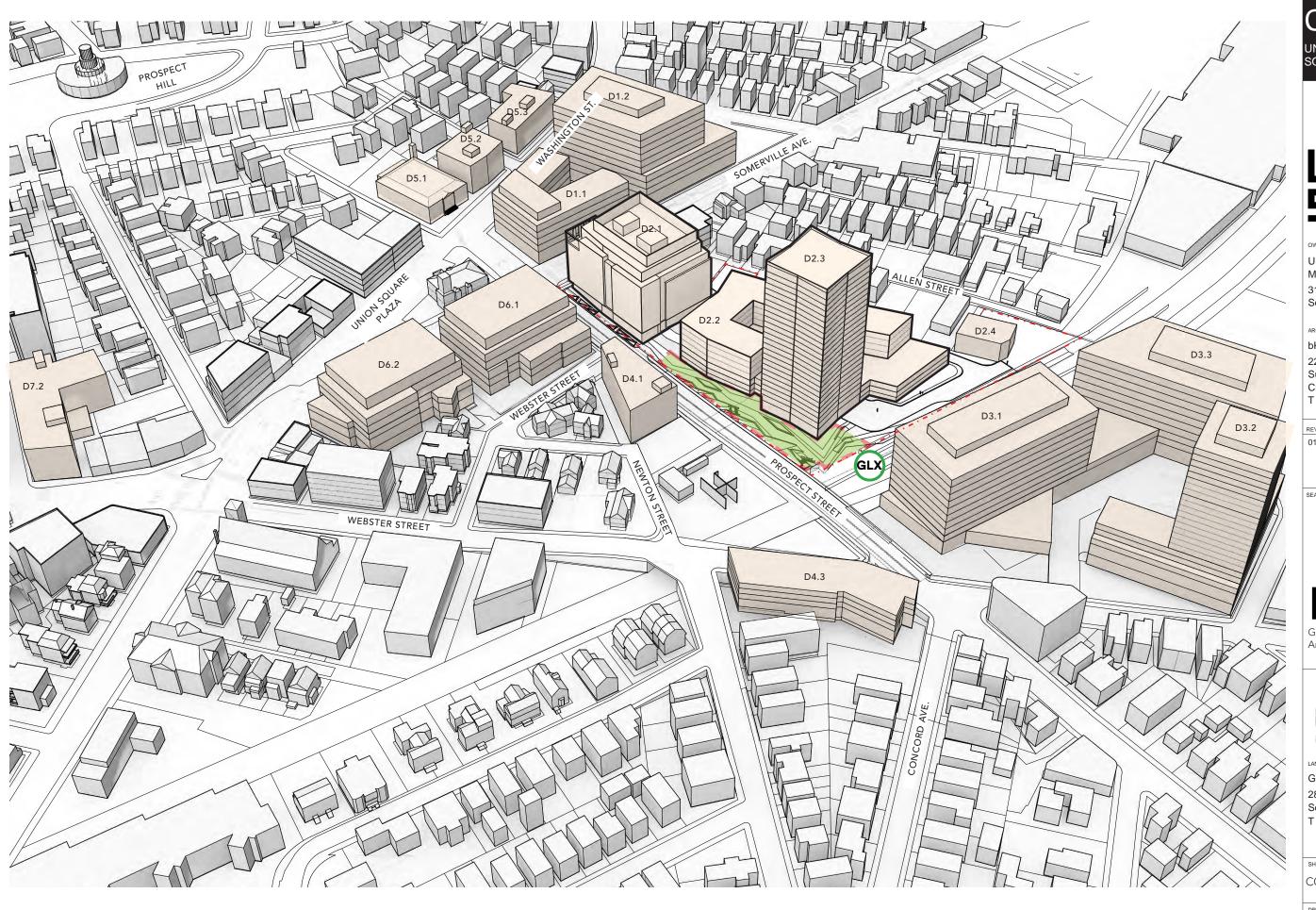
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CONTEXT ANALYSIS - EXISTING



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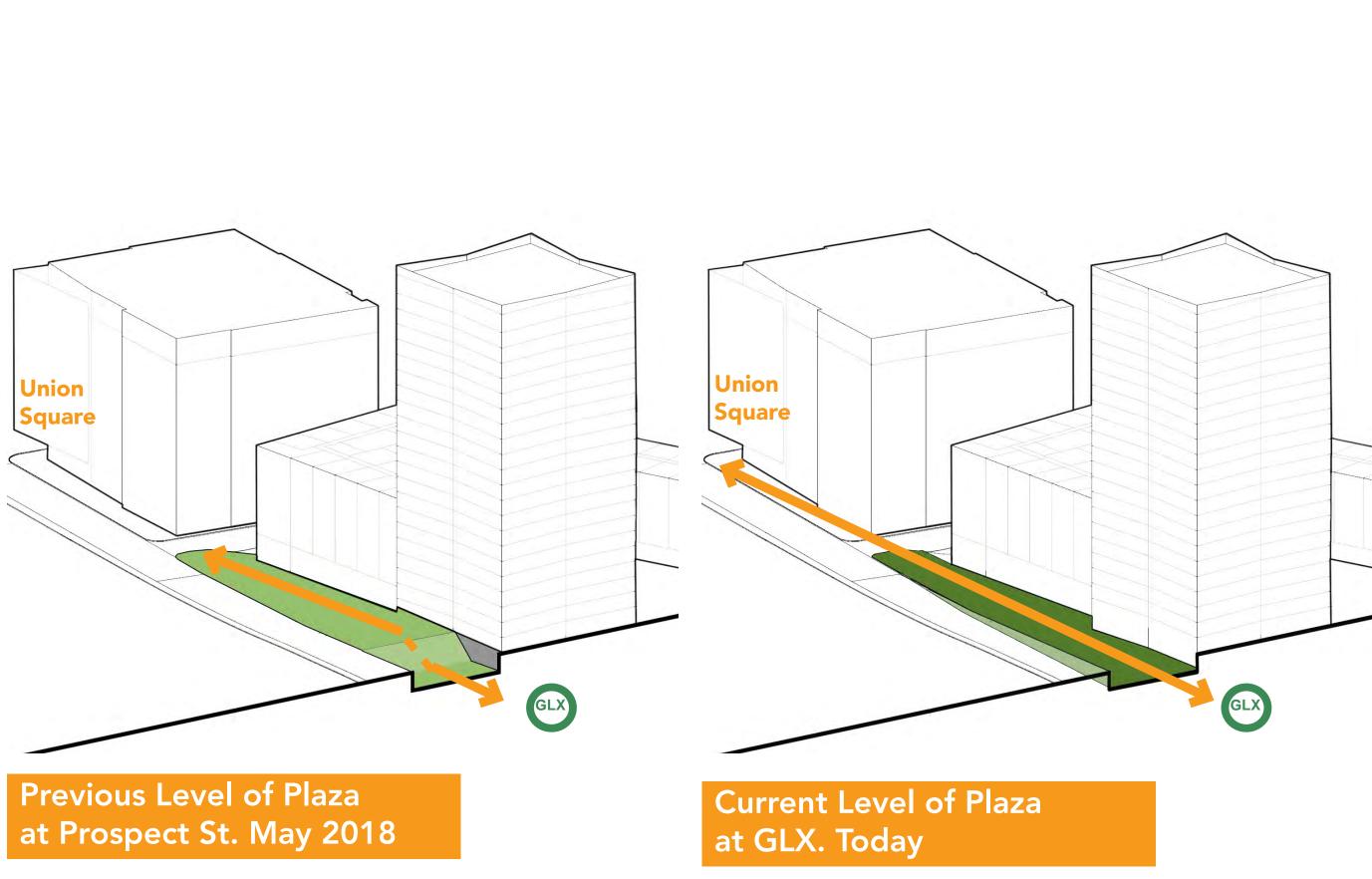
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CONTEXT ANALYSIS - FUTURE



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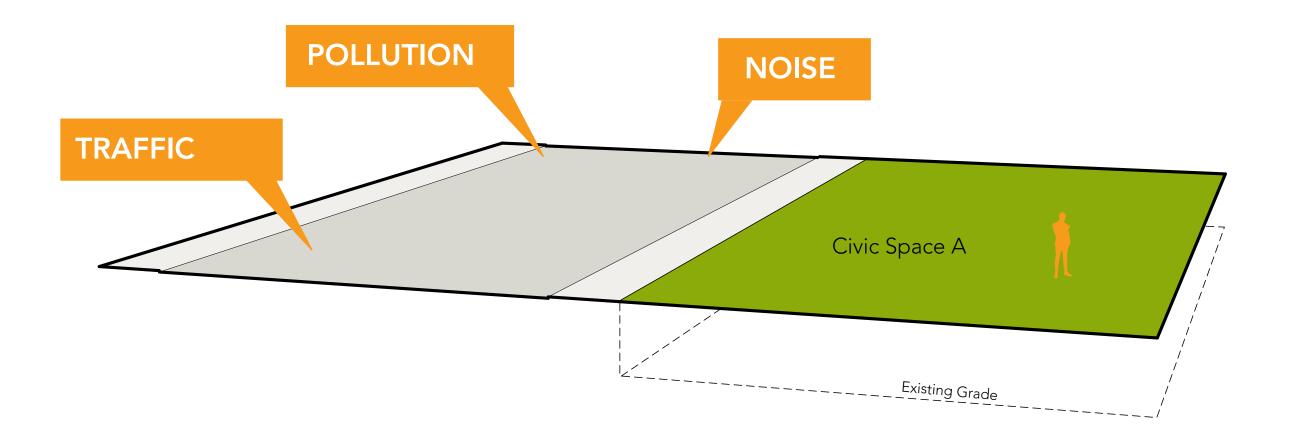
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SHEET

CIVIC SPACE DESIGN -PLAZA LEVEL



ADJACENCY CHALLENGE - HOW TO CREATE A GREAT PEDESTRIAN STREET AND SUCCESSFUL CIVIC SPACE?

## Civic Space A

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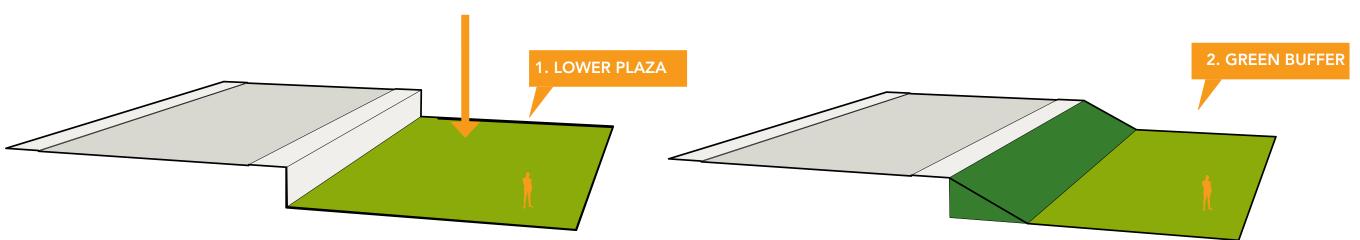
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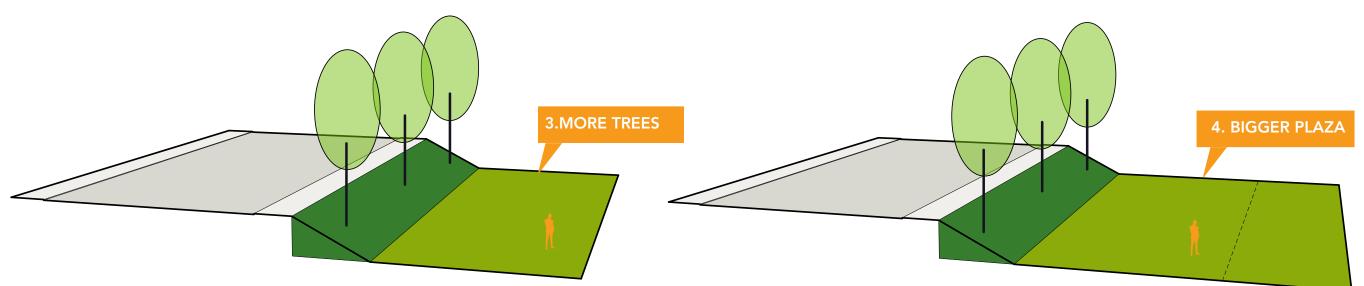
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CIVIC SPACE DESIGN -CHALLENGE



The civic space is lowered to create a vertical separation between the street and plaza.

A green buffer creates protection from Prospect Street noise and pollution.



Verticality of trees defines 'outdoor room' and privacy from Prospect Street

Increased overall plaza area generates more green and active space.

**SOLUTION** - LEVERAGE EXISTING GRADE CHANGE AND ADD GREEN BUFFER TO DEFINE OUTDOOR ROOM.

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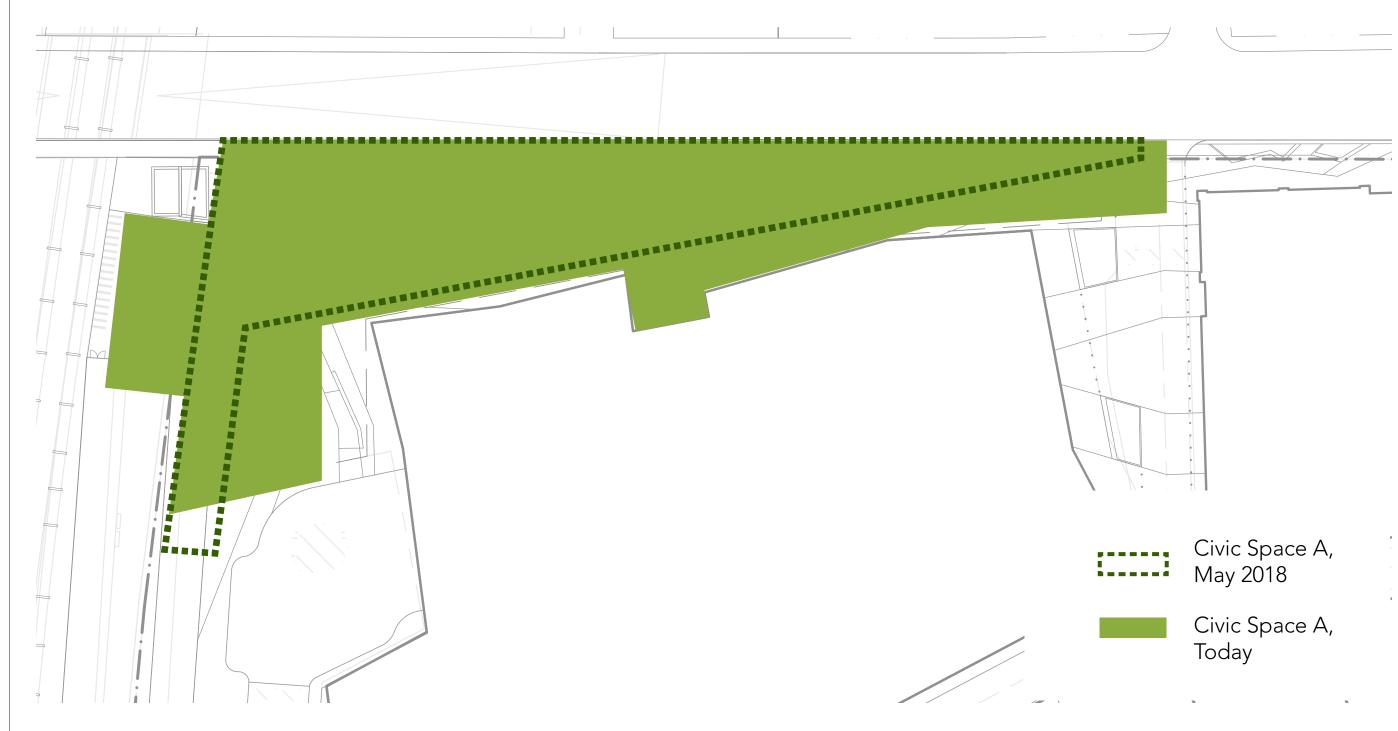
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CIVIC SPACE DESIGN -SOLUTION

# PROVIDING MORE HIGH-QUALITY CIVIC SPACE: CIVIC SPACE AREA INCREASES OVER 25%



# Civic Space A

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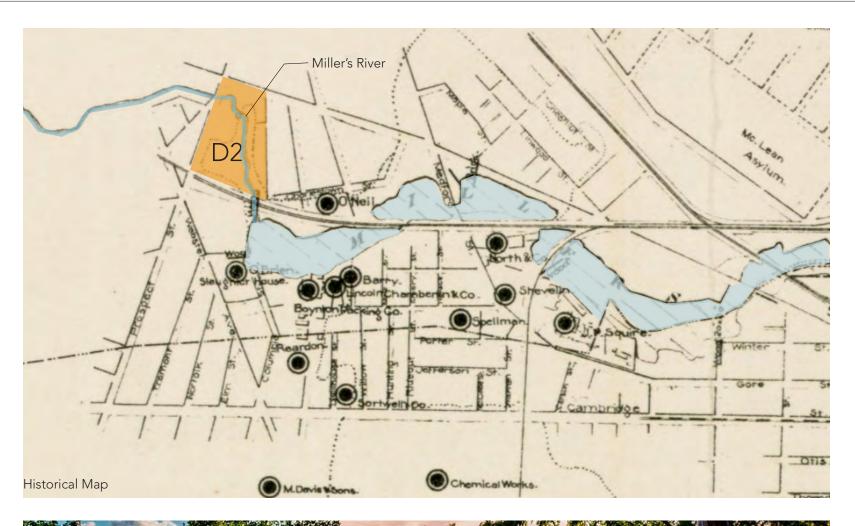
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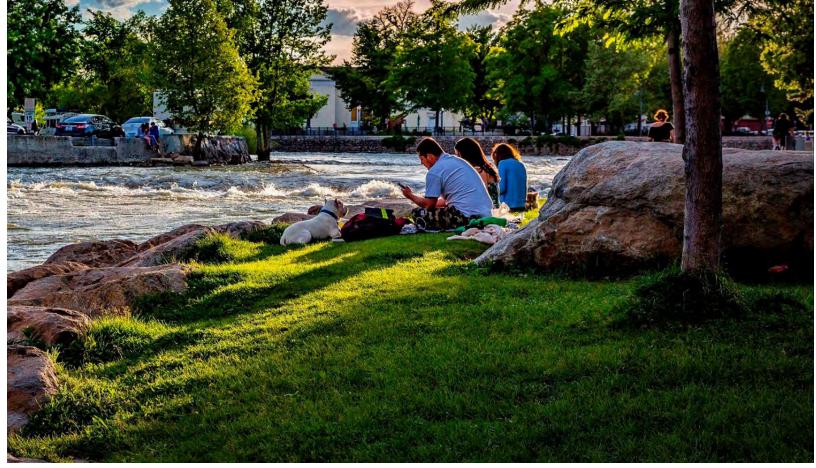
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SHEET

CIVIC SPACE DESIGN -INCREASE GREEN





# LANDSCAPE CONCEPT RECALLING MILLER'S RIVER:

A New Plaza that Connects GLX to the heart of Union Square

# Civic Space A UNION SQUARE SOMERVILLE, MA



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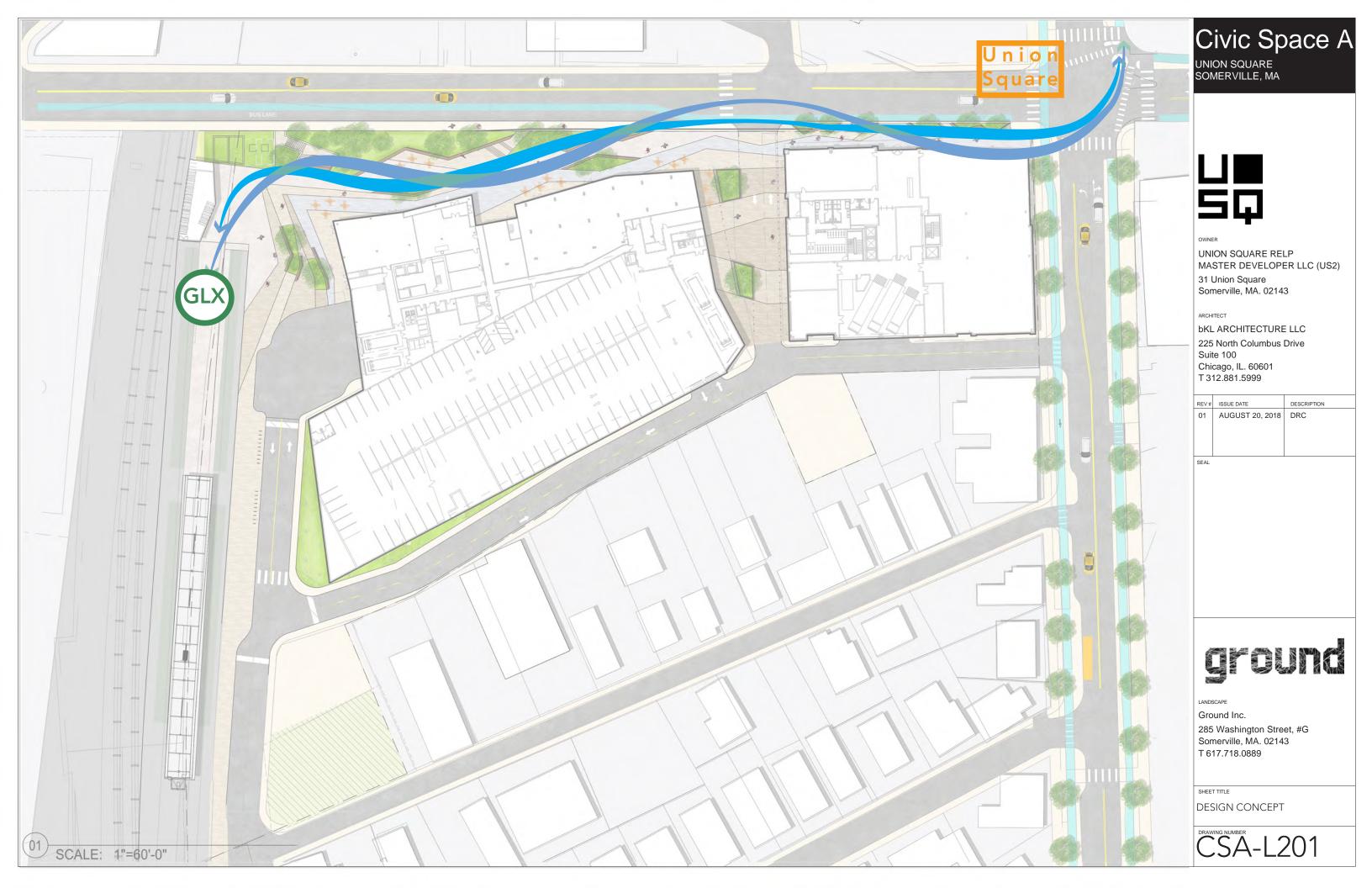
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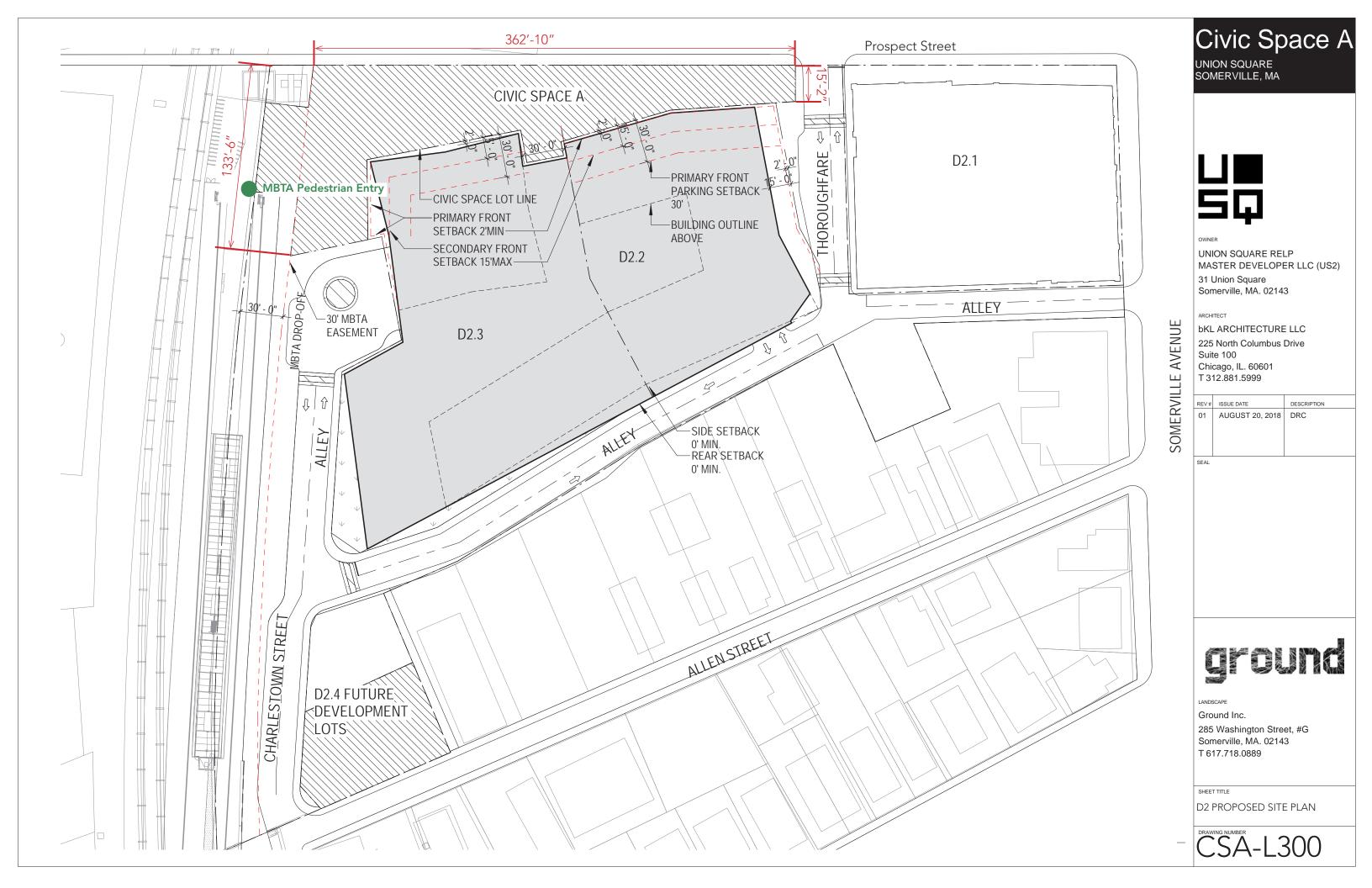
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SHEET TIT

DESIGN CONCEPT







### A GATEWAY PLAZA FOR UNION SQUARE

- 1 Sitting Steps
- 2 Green Buffer
- 3 Stairs
- 4 Planter
- 5 Memory of the River
- 6 Electrical Room and Transformer
- 7 Bike Shelter [Potential Public Art Location]





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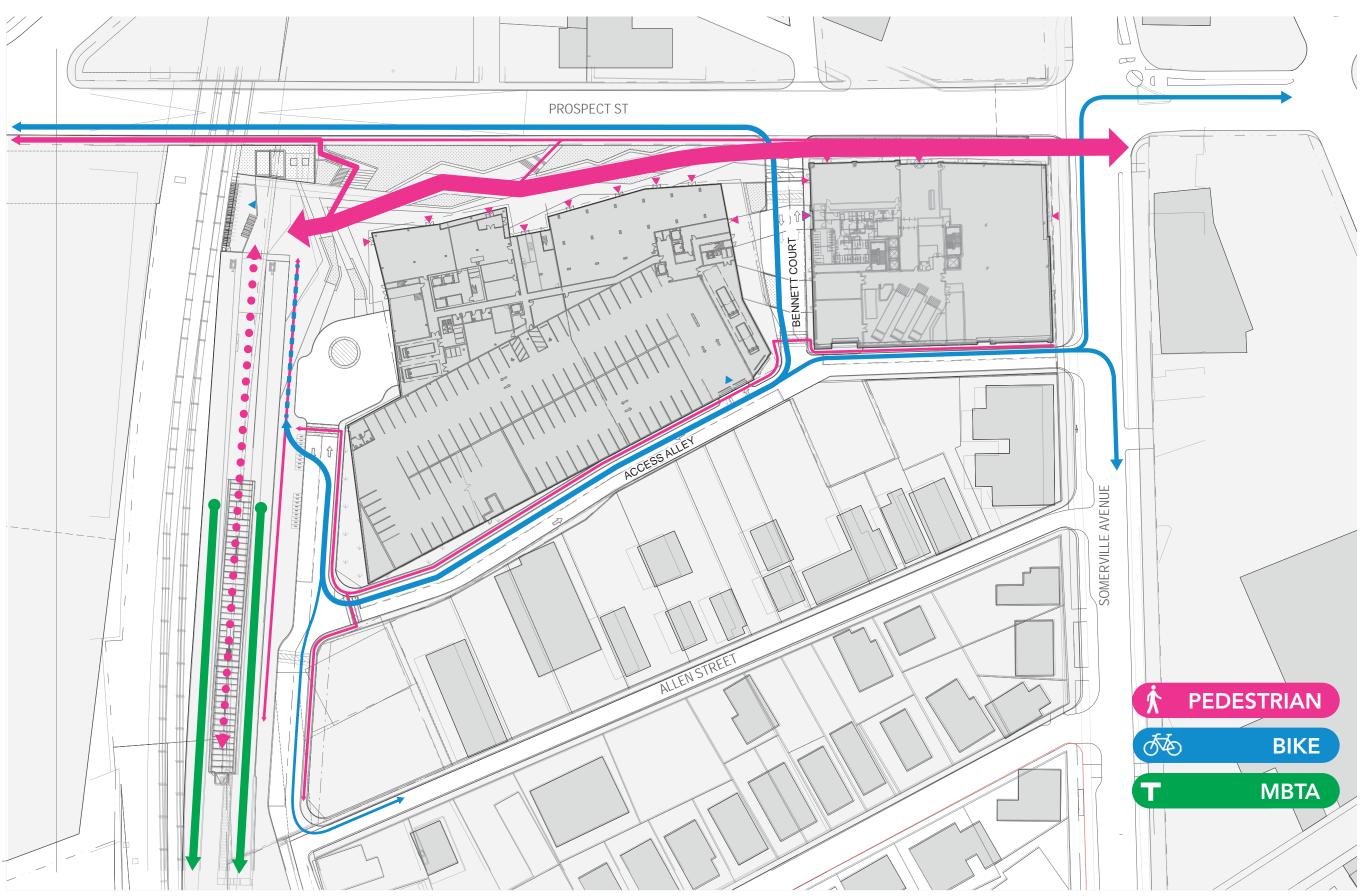
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ILLUSTRATIVE CIVIC SPACE A PLAN



## CONNECTIONS FOR PEDESTRIANS AND BICYCLES



## Civic Space A

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PEDESTRIAN CIRCULATION



## MULTIPLE AND VARIED ACTIVITY ZONES





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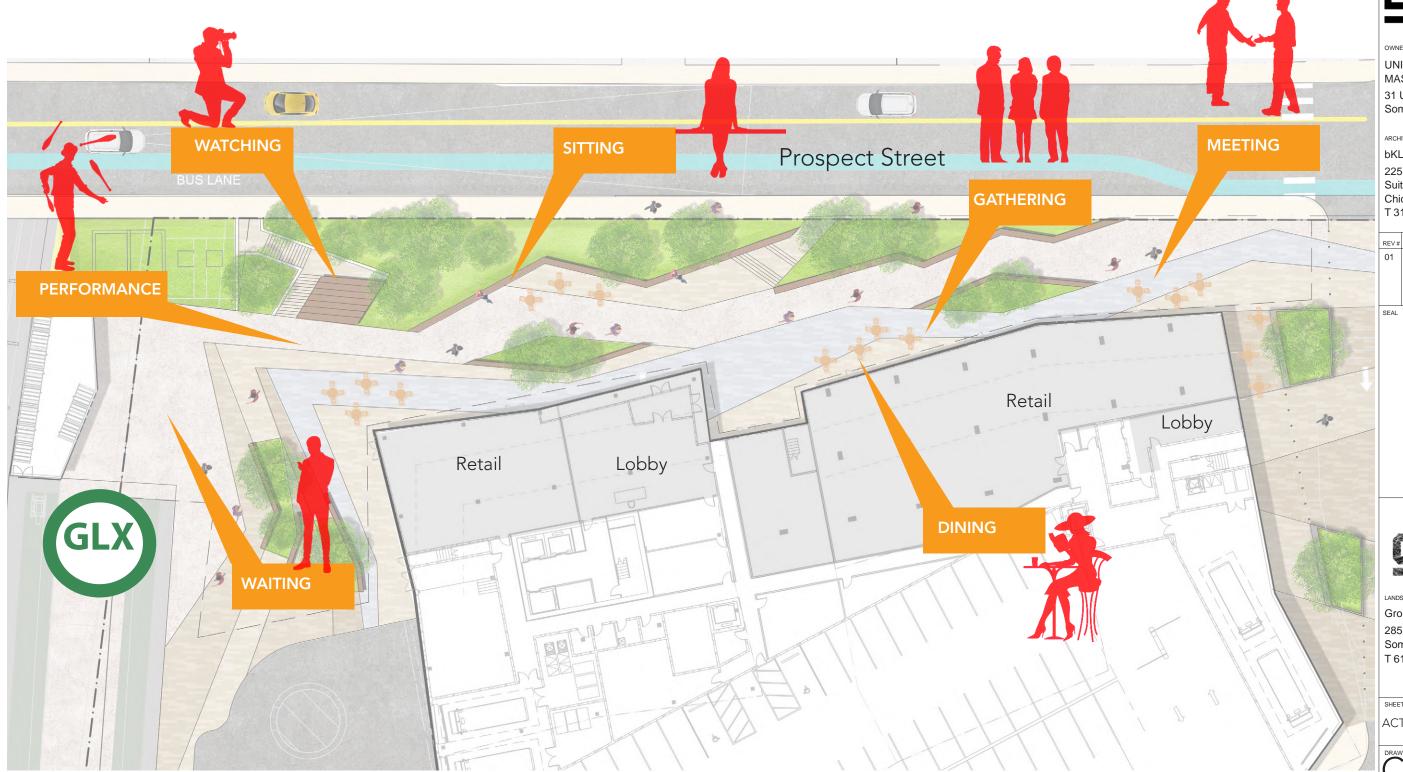
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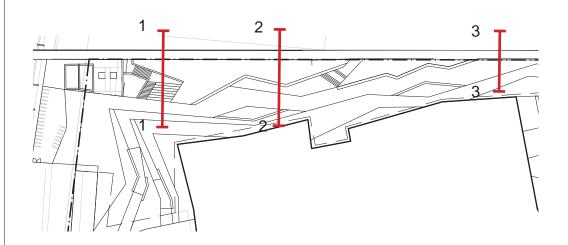
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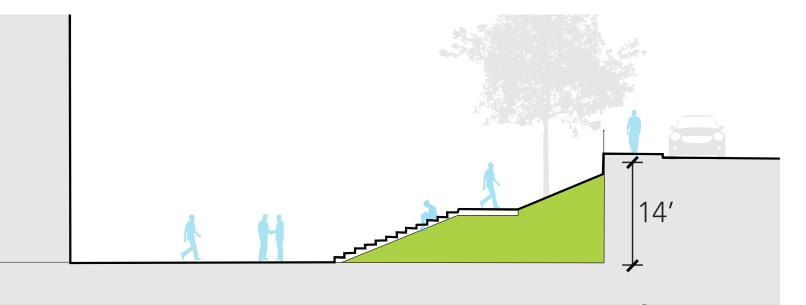
ACTIVITIES

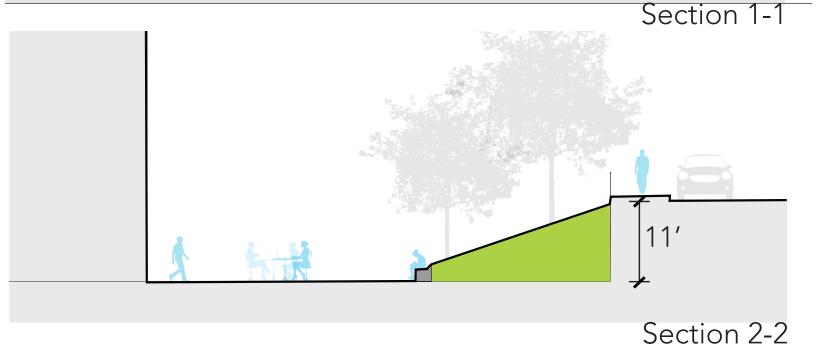


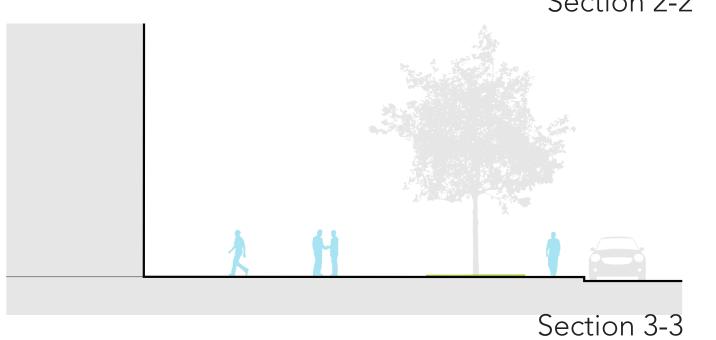


## Site Sections









## Civic Space A

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PLAZA SECTIONS



UNION SQUARE SOMERVILLE, MA



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SHEET TITLE
PERSPECTIVE VIEW
- TOWARD GLX STATION



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SHEET TIT

PERSPECTIVE VIEW
- ARRIVING UNION SQUARE



UNION SQUARE SOMERVILLE, MA



OWNER

UNION SQUARE RELP MASTER DEVELOPER LLC (US2) 31 Union Square Somerville, MA. 02143

ARCHITEC'

bKL ARCHITECTURE LLC 225 North Columbus Drive Suite 100 Chicago, IL. 60601 T 312.881.5999

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PERSPECTIVE VIEW
- TOWARDS UNION SQUARE

## Table 6.7.9 Civic Space Standards

|                                      | Neighborhood<br>Park | Pocket<br>Park               | Green                        | Plaza   | Through<br>Block<br>Plaza    | Pocket<br>Plaza   | Playground                 | Dog Park                           | Community<br>Garden |
|--------------------------------------|----------------------|------------------------------|------------------------------|---|------------------------------|---|----------------------------|------------------------------------|---------------------|
| Lot Size<br>(min)                    | 8,000 sf.            | 800 sf.                      | 6,000 sf.                    | 8,000 sf.   | 5,000 sf.                    | 2,000 sf.   | 2,500 sf.                  | 10,000 sf.                         | 4,000 sf.           |
| Lot Size<br>(max)                    | 2 acres              | 10,000 sf.                   | 1 acre                       | 2 acres   | 10,000 sf.                   | 10,000 sf.  | 25,000 sf.                 | -                                  | 15,000 sf.          |
| Seating<br>(min)                     | n/a                  | 1 linear<br>foot / 75<br>sf. | 1 linear<br>foot / 75<br>sf. | 1 linear<br>foot / 30<br>sf.                              | 1 linear<br>foot / 75<br>sf. | 1 linear<br>foot / 50<br>sf.                              | 1 linear foot<br>/ 300 sf. | 1 linear<br>foot / 750<br>sf.      | n/a                 |
| Tables<br>(min)                      | n/a                  | n/a                          | n/a                          | 1 per<br>every 4<br>movable<br>chairs<br>(if<br>provided) | n/a                          | 1 per<br>every 3<br>movable<br>chairs<br>(if<br>provided) | n/a                        | n/a                                | n/a                 |
| Number of<br>Large<br>Trees<br>(min) | 1 / 1,225 sf.        | 1 / 400 sf.                  | 1 / 1,225<br>sf.             | 1 / 1,000<br>sf.  | 1 / 1,500<br>sf.             | 1 / 400 sf.   | 1 / 750 sf.                | 1 / 30 ft.<br>of front lot<br>line | n/a                 |
| Pervious<br>Area (min)               | 85%                  | 85%                          | 85%                          | 70%   | 70%                          | 70%   | 85%                        | 100%                               | 85%                 |
| Green<br>Space<br>(min)              | 50%                  | 70%                          | 70%                          | 10%   | 10%                          | 10%   | 30%                        | 10%                                | 70%                 |

# Civic Space A UNION SQUARE SOMERVILLE, MA



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SHEET TITLE

CIVIC SPACE CODE



MASTER DEVELOPER LLC (US2)

225 North Columbus Drive

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### TOTAL PLAZA: 19,500 SF

### Major Plaza Area = 14,625 SF

(01)

Scale: 1"=30'-0"

75% of total civic space area

Prospect Frontage Length = 202'

Max Length = 3x average depth (68') = 204'

### Minor Plaza

Area =  $4,875 \, SF$ 25% of total civic space area

## Civic Space A UNION SQUARE SOMERVILLE, MA



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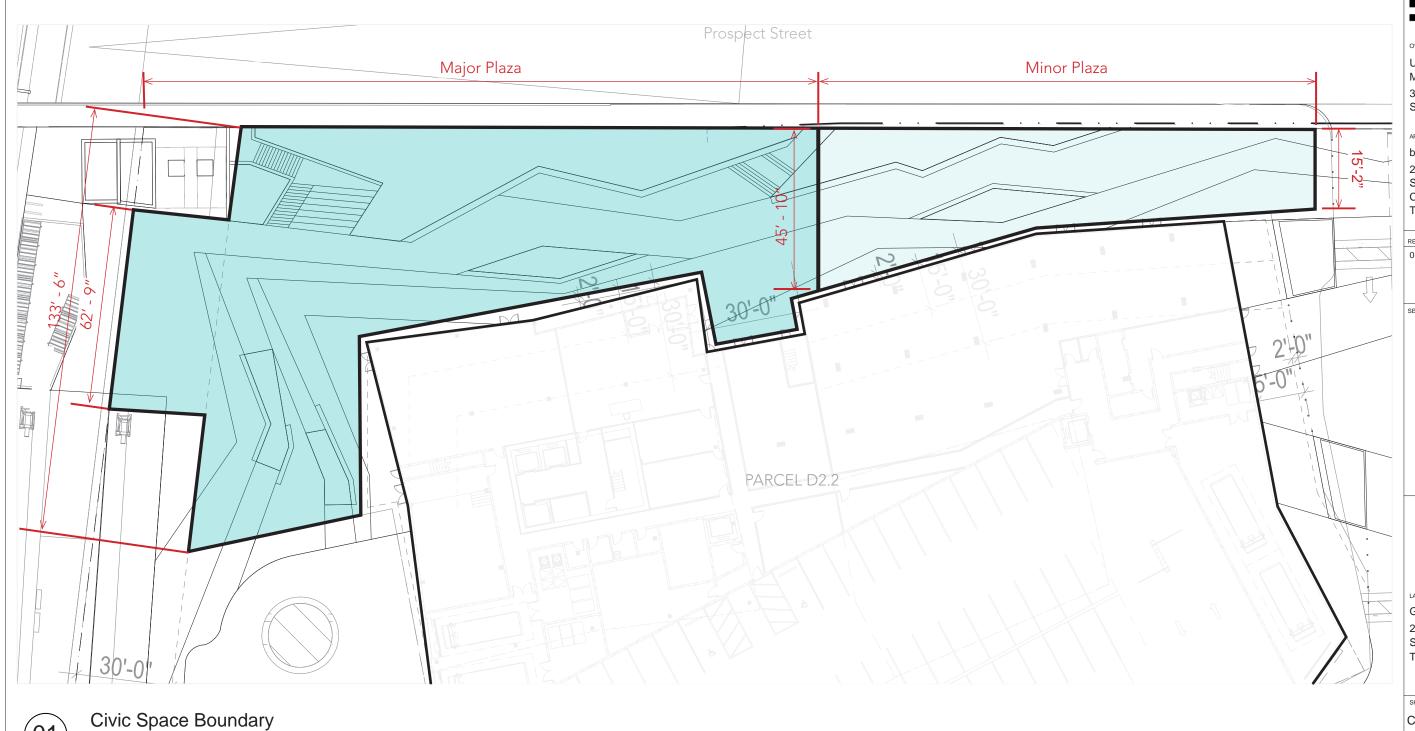
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CIVIC SPACE BOUNDARY

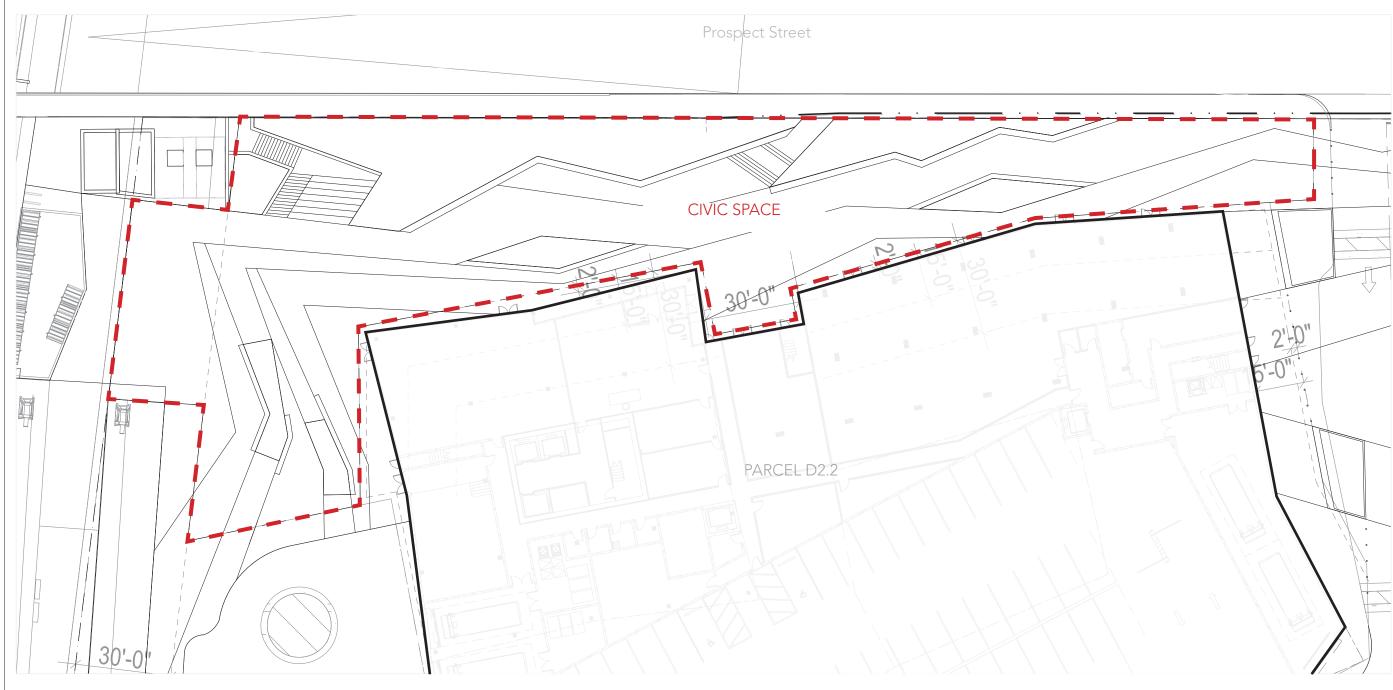


### FRONTAGE ZONE- EXEMPT:

Frontage Zone

Scale: 1"=30'-0"

The first fifteen feet of a plaza measured perpendicularly from any front lot line is considered the frontage area of a plaza. Plazas with a front lot line abutting the wing wall of a bridge approach are exempt.



Civic Space A

UNION SQUARE SOMERVILLE, MA



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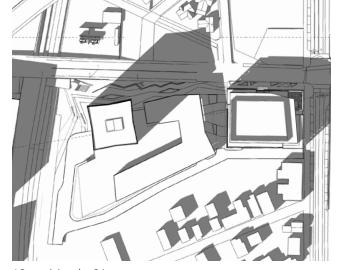
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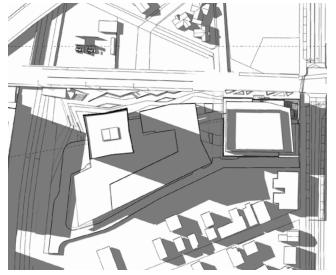
FRONTAGE ZONE



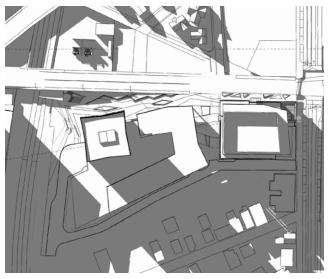
9am March 21



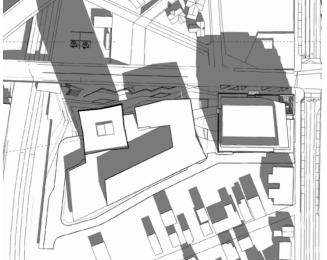
12pm March 21



3pm March 21



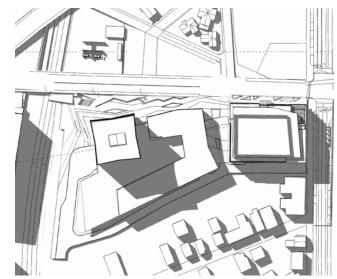
5pm March 21



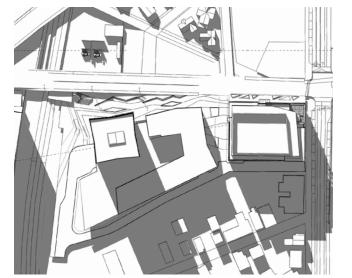
9am June 21



12pm June 21



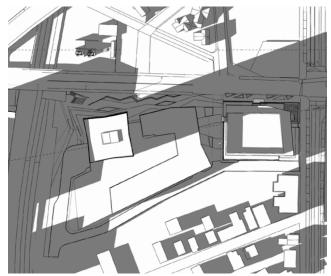
3pm June 21



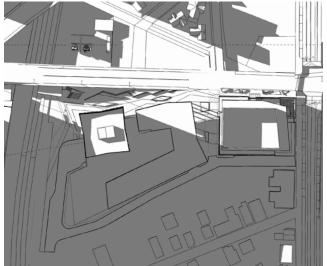
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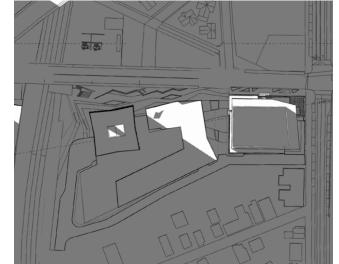
9am December 21



12pm December 21



3pm December 21



5pm December 21



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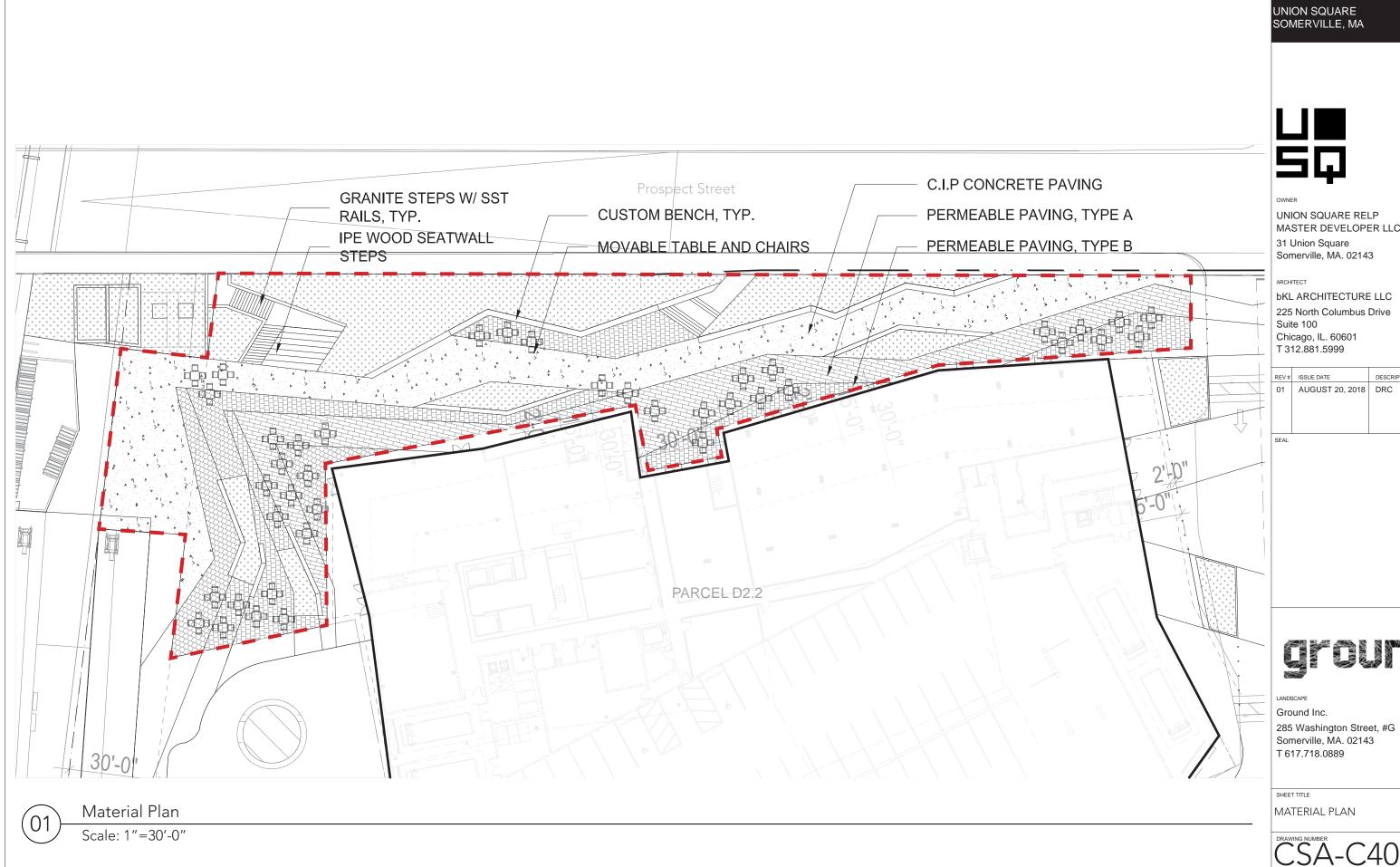
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SOLAR STUDY



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SHEET TITLE

GREEN AREA

### Pervious Area

Required: 19,500 SF X 70 % = 12,650 SF

Designed: 11,800 SF\*

### Requirement:

- 70% of the overall Civic Space area must be permeable.
  - \* Subject to ongoing coordination of material specification for MBTA's accessible path



### Pervious Area

Scale: 1"=30'-0"

## Civic Space A

UNION SQUARE SOMERVILLE, MA



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RCHITECT

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PERVIOUS AREA





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SEAL

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PERVIOUS MATERIAL

CSA-C601

Pervious Material

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Civic Space A

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OUTDOOR SEATING AND FURNISHINGS

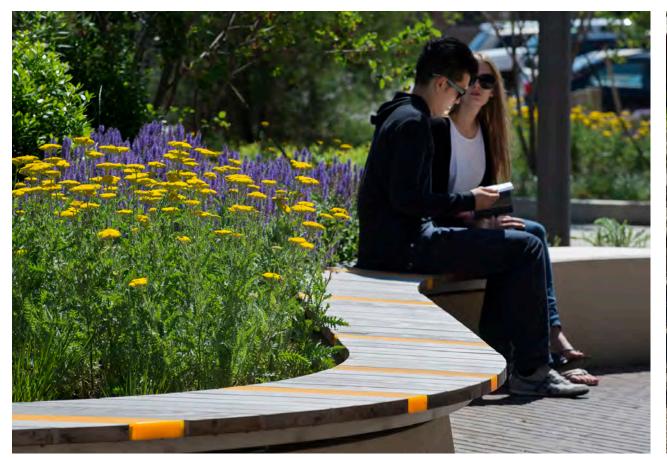
CSA-C700

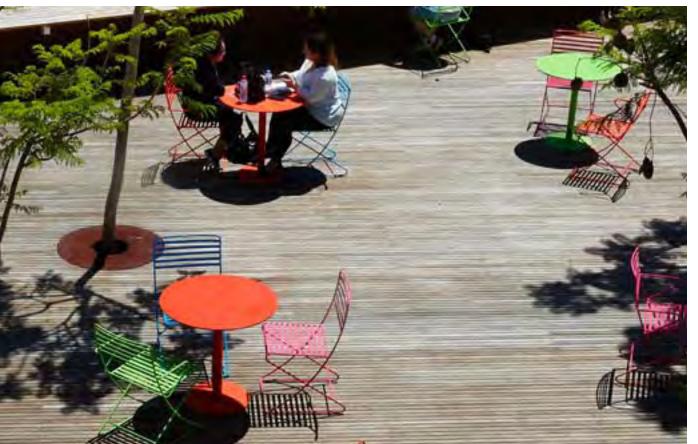
Seating Requirement: 1 linear foot of required seating for every 30 SF of Civic Space area
Seatwalls may only account for 15% of the overall required seating Required: 650 LF • 1 table is required for every 4 movable chairs • 1 movable chairs is 2 LF Seating 699 LF Designed: 105 LF of Seatwall (120 LF available that can not be counted towards required total) 265 LF of Bench 41 Movable Tables and Chairs Prospect Street RHODGE -

PARCEL D2.2

Outdoor Seating

Scale: 1"=30'-0"









UNION SQUARE SOMERVILLE, MA



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SHEET TITLE

OUTDOOR SEATING

CSA-C701

Outdoor Seating

#### CIVIC SPACE DESIGN GUIDELINES

6.7.5.D.4.c.ii.b.2.a:

#### General Design Review Criteria- Civic Spaces

The appropriateness of the proposed design for the site, with specific regard solar orientation, integration with nearby buildings, and type and use of other nearby

For solar orientation, see page CSA-C300
The site has exposure to the sun in the afternoon and early evening. The siting of the civic space is consistent with the requirements stated in the Neighborhood Plan and pedestrian orientation for lots with frontage on Prospect Street.

For integration with nearby building and type and use of other nearby civic space, see page CSA-L200. The surrounding buildings and civic spaces have different program type from Civic Space A Plaza. Civic Space A has an important role acting as a connection between the existing Union Square to the proposed GLX Station.

6.7.5.D.4.c.ii.b.2.b:

#### General Design Review Criteria- Civic Spaces

The maximization of the actual utility of a civic space for its intended use through landscape design and accommodation of pedestrian desire lines.

For utility of civic space for its intended use through landscape design and accommodation of pedestrian desire lines, see pages CSA-L600 and CSA-L601. A clear path has been maintained for unobstructed circulation between Union Square and the GLX Station. The paths to all major building entrances have also been designed to accommodate direct access.

#### 6.7.5.D.4.c.ii.b.2.c:

#### General Design Review Criteria- Civic Spaces

The appropriateness of irrigation and drainage systems to effectively reduce water use, minimize or eliminate storm water runoff into the sewer or storm system, address unique site conditions and meet the specific needs of the type of civic space.

For irrigation and drainage systems, see page CSA-L800
The majority of the Civic Space surface has been designed to be permeable in order to minimize surface runoff. All plant species selected are drought tolerant, requiring a low-water use irrigation system.

#### 6.7.5.D.4.c.ii.b.2.c:

#### General Design Review Criteria- Civic Spaces

A plaza should contain substantial areas of hardscape complemented by planting beds or arrangements of trees with open canopies.

Response

i. For plaza design, see page CSA-L501 and CSA-C400 An open hardscape area is framed by plant beds to create a flexible space to for programming and circulation.

ii. Plantings should contribute to the spatial definition of the space as an outdoor room

ii. For planting design, see page CSA-L501 and CSA-C500 Outdoor rooms have been created by using a unique planter shape and designing seating/areas for gathering.

iii. Benches and seating ledges or walls should be designed for the convenience and comfort of visitors, located in support of gathering spaces and along pedestrian circulation paths, but should be out of the flow of pedestrian traffic.

riii. For seating layout, see page CSA-C700 and CSAC701

The plaza uses a variety of seating elements including seatwalls, benches, movable chairs and seat steps to create a variety of gathering spaces. Seat steps have been sited to facilitate flexible programing of open space and accommodate performances and social gathering. All seating has been kept out of the main circulation path joining Union Square to the GLX Station.

iv. The perimeters of a plaza should be well integrated into its surroundings and free from fences, hedges and other barriers that would impede movement into the space and obscure visibility from adjacent streets or building frontages.

For plaza design, see page CSA-L501 and CSA-L900

The perimeter of the plaza has been integrated to the surrounding street by designing a connective green buffer that can be enjoyed from both sides. Lower planting will be used to maintain clear sight lines between the plaza and the adjacent properties. Multiple points of entry invite movement into the space. No additional fences

## Civic Space A

UNION SQUARE SOMERVILLE, MA



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Somerville, MA. 02143

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DESIGN REVIEW CHECKLIST